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Jesse Hoover
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 DN: c=US, st=Tennessee, l=Nashville, o=TDOT, ou=TDOT,
 cn=Jesse Hoover, email=Jesse.Hoover@tn.gov
 Date: 2023.09.27 13:43:17 -0500

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TENNESSEE DEPARTMENT OF TRANSPORTATION
 6601 CENTENNIAL BLVD.,
 NASHVILLE, TN 37243
 JESSE HOOVER, P.E. NO.120378

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD DRAWINGS.....	1A
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2A
GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C, 2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES.....	2E

YEAR	PROJECT NO.	SHEET NO.
2023	NH-I-24-1(140)	ROADWAY-SIGN1

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

SIGNATURE
SHEET

27-SEP-2023 10:29 \\tdo\03nas002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Interstate\Davidson Co. I-24 L.M. 17.33 - 21.10\10-131712-00-SignatureSheet.sht



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TENNESSEE DEPARTMENT OF TRANSPORTATION
 6601 CENTENNIAL BLVD.,
 NASHVILLE, TN 37243
 JESSE HOOVER, P.E. NO.120378

2-5-24

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET.....	ROADWAY-SIGN2
TITLE SHEET	1
ROADWAY INDEX AND STANDARD DRAWINGS.....	1A
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2A
GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C, 2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES.....	2E

YEAR	PROJECT NO.	SHEET NO.
2024	NH-I-24-1(140)	ROADWAY-SIGN2

REV. 01-23-24: ADDED SHEET TO PLANS

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

SIGNATURE
 SHEET

23-JAN-2024 11:50 \\tdo\03nas002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Interstate\Davidson Co. I-24 L.M. 17.33 - 21.10\10-131712-00-SignatureSheet2.sht



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Jesse Hoover
DN: c=US, st=Tennessee, ln=Nashville, o=TDOT, ou=TDOT, cn=Jesse Hoover, email=Jesse.Hoover@tn.gov, Date: 2024.02.22 14:32:39 -06'00'

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SHEET NAME	SHEET NO.
SIGNATURE SHEET.....	ROADWAY-SIGN3
ROADWAY INDEX AND STANDARD DRAWINGS.....	1A
ESTIMATED ROADWAY QUANTITIES.....	2
TABULATED QUANTITIES.....	2E

YEAR	PROJECT NO.	SHEET NO.
2024	NH-I-24-1(140)	ROADWAY-SIGN3

REV. 02-21-24: ADDED SHEET TO PLANS

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

SIGNATURE
 SHEET

21-FEB-2024 07:59 \\tdo\03nas002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Interstate\Davidson Co. I-24 L.M. 17.33 - 21.10\10-131712-00-SignatureSheet3.sht

INDEX OF SHEETS

SEE SHEET 1A

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF ENGINEERING

DAVIDSON COUNTY

INTERSTATE 24
FROM NEAR I-440 (L.M. 17.33)
TO NEAR HAYWOOD LANE (L.M. 21.10)

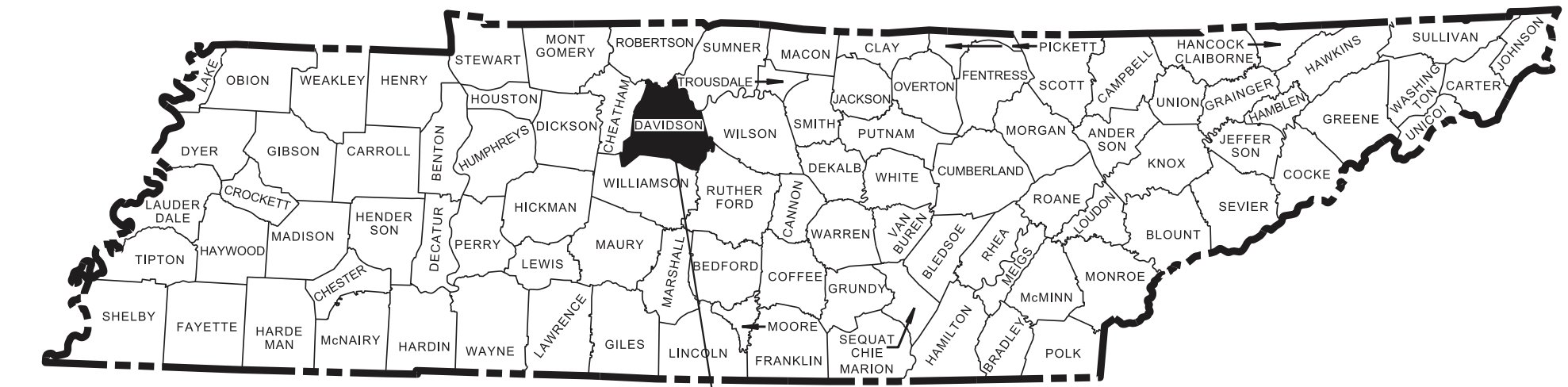
RESURFACE
COLD PLANING, RESURFACING, PAVEMENT MARKING

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-24

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2024	1
FED. AID PROJ. NO.	NH-I-24-1(140)	
STATE PROJ. NO.	19I024-F8-008	

REV. 01-23-24: UPDATED YEAR ON ALL SHEETS.



PROJECT LOCATION

BRIDGE ID. # 19I00240011 19I00240013 19I00240015
 19I00240017 19I00240021 19I00240023
 19I00240025 19I00240027

NO EXCLUSIONS

19I024-F8-008
 BEGIN PROJECT NO. NH-I-24-1(140) RESURFACE
 L.M. 17.33

CSXT [349217F] UNDERPASS CROSSING #19I00240011
 CSXT M.P. J 0004.580
 L.M. 17.56

CSXT [340974V] OVERHEAD CROSSING #19I00240025
 CSXT M.P. DL 0003.170
 L.M. 19.38

19I024-F8-008
 END PROJECT NO. NH-I-24-1(140) RESURFACE
 L.M. 21.10

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT ROAD SP. SV. 2 : JESSE HOOVER, P.E.

DESIGNER : CAYLIE STOCKTON CHECKED BY : KONRAD BECK

P.E. NO. 98034-4194-00 (DESIGN)

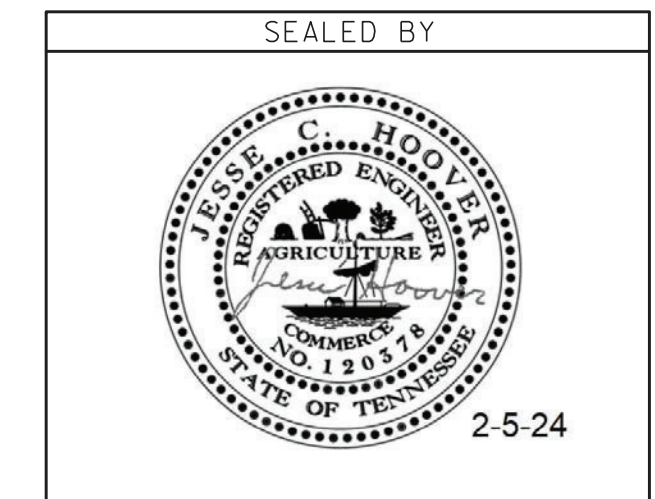
PIN NO. 131712.00



SCALE: 1" = 1 MILE



PROJECT LENGTH 3.77 MILES
 TOTAL LANE MILES RESURFACED 33.28 MILES



APPROVED: *Will Reid*
 WILL REID, CHIEF ENGINEER

DATE: _____

APPROVED: *Howard H. Eley*
 HOWARD H. ELEY, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION

TRAFFIC DATA	
ADT (2024)	181,270
POSTED SPEED	
L.M. 17.33 - 18.71	55 MPH
L.M. 18.71 - 20.13	65 MPH
L.M. 20.13 - 21.10	70 MPH

APPROVED: _____
 DIVISION ADMINISTRATOR DATE

23-JAN-2024 11:23 \\tdot03nas002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Interstate\Davidson Co. I-24 L.M. 17.33 - 21.10\1-131712-00-TitleSheet.dgn

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEETS.....	ROADWAY-SIGN1-3
TITLE SHEET	1
ROADWAY INDEX AND STANDARD DRAWINGS.....	1A
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE.....	2A
GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C, 2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES.....	2E
DETAIL SHEET.....	2F
PAVEMENT MARKING IMPROVEMENTS.....	2G – 2G7

NOTE:
 NO PROJECT COMMITMENTS SHEETS INCLUDED IN THIS SET OF PLANS.
 NO UTILITY SHEETS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
DESIGN - TRAFFIC CONTROL		
T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-5	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAYS
T-M-6	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	06-28-19	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	06-28-19	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-9	05-01-23	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9A		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-M-18A		DELINEATOR MOUNTING DETAILS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-13	05-01-20	TWO-OUTSIDE LANE CLOSURE FOR EXPRESSWAY AND FREEWAYS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63		WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	1A

REV. 01-23-24: ADDED ROADWAY-SIGN2 TO THE ROADWAY INDEX.

REV. 02-21-24: ADDED ROADWAY-SIGN3 TO THE ROADWAY INDEX.
 ADDED STANDARD ROADWAY DRAWING T-M-18A.

SEALED BY



2-22-24

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
 AND
 STANDARD
 DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2

REV. 01-23-24: REMOVED ITEMS 716-02.07 AND 716-12.03. ADDED ITEMS 716-15.11 AND 716-15.20. UPDATED FOOTNOTE (19).

REV. 02-21-24: UPDATED ITEM 604-04.02. UPDATED FOOTNOTE (17).

ESTIMATED ROADWAY QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY 191024-F8-008
	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M. 15.08
(1)	303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON 879
(2)	307-03.08	ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING B-M2	TON 500
	307-03.11	ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING CW	TON 23722
(3)	403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON 145
(4)	411-03.10	ACS MIX(PG76-22) GRADING D	TON 3320
(5)(6)(7)	411-03.23	ACS MIX (PG76-22) OGFC	TON 16053
	411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M. 15.08
(8)	415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON 23065
(17)	604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y. 6963
(22)	611-03.04	GRAY IRON CASTINGS (CATCHBASIN)	LB. 670
(9)	712-01	TRAFFIC CONTROL	LS 1
	712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH 200
(10)	712-06	SIGNS (CONSTRUCTION)	S.F. 1544
	712-08.03	ARROW BOARD (TYPE C)	EACH 2
	712-08.08	SPEED FEEDBACK SIGN ASSEMBLY	EACH 2
	712-08.09	DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH 6
	712-08.12	QUEUE PROTECTION TRUCK	DAY 120
	713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH 2
(21)	716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH 126
	716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH 1954
	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH 1900
	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F. 96
(11)	716-04.07	PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH 20
(11)	716-04.08	PLASTIC PAVEMENT MARKING (OPTION LANE ARROW)	EACH 8
(11)	716-04.09	PLASTIC PAVEMENT MARKING (H.O.V. DIAMOND)	EACH 8
	716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F. 26
(11)	716-04.14	PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH 10
(12)(13)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M. 18.2
	716-09.94	CONTRAST PAVEMENT SHADOW MARKING 6"	L.M. 0.7
(13)(14)(20)	716-10.50	PREFORMED PLASTIC PAVEMENT MARKING (INTERSTATE SHIELD)	EACH 10
	716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M. 16.2
(20)	716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F. 6500
(20)	716-12.09	ENHANCED FLAT LINE THERMO (12IN LINE)	L.F. 7620
(20)	716-12.10	ENHANCED FLAT LINE THERMO (12IN DOTTED)	L.F. 5100
(19)	716-15.11	PREFORMED PERMANENT TAPE (8IN LINE)	L.F. 6200
(19)	716-15.20	PREFORMED PERMANENT TAPE (24IN BARRIER LINE)	L.F. 2600
	717-01	MOBILIZATION	LS 1
	730-03.20	INSTALL PULL BOX (TYPE A)	EACH 2
	730-12.02	CONDUIT 2" DIAMETER (PVC)	L.F. 40
	730-14.02	SAW SLOT	L.F. 750
(18)	730-14.03	LOOP WIRE	L.F. 3000
		ALTERNATE AA1	
(15)	403-02.01	TRACKLESS TACK COAT	TON 249
		ALTERNATE AA2	
(16)	403-02.02	HOT APPLIED TACK COAT	TON 184

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER.
- (2) TO BE USED AS DIRECTED BY THE ENGINEER FOR SPOT LEVELING.
- (3) INCLUDES 5 TONS FOR RAMPS.
- (4) TO BE USED FOR GORES, RAMPS, EXIT LANES, AND TRANSITIONS AT BRIDGES AS SHOWN IN PAVEMENT DETAIL ON SHEET 2B.
- (5) MODIFY AIR VOID CONTENT SPECIFIED IN TABLE 411.03-04 FROM MINIMUM 20% TO MINIMUM 17% AS DETERMINED BY THE "VOLUME METHOD" DESCRIBED IN SECTION 6.2.2 OF AASHTO T 269. AGE CANTABRO TEST SPECIMENS FOR 4 HOURS AT LAB COMPACTION TEMPERATURE. TSR TESTING SHALL BE PERFORMED ACCORDING TO THE METHOD DESCRIBED IN 407.03.E.1 FOR OGFC AND MEET A MINIMUM TENSILE STRENGTH OF 50 PSI AND A MINIMUM TSR OF 70%.
- (6) 238 TONS TO BE USED FOR PLANT STARTUP AND HEATING UP EQUIPMENT AT BEGINNING OF EACH SHIFT, 15 TONS WASTE MATERIAL PER DAY.
- (7) TSR TESTING SHALL BE PERFORMED ACCORDING TO THE METHOD DESCRIBED IN 407.03 E1 FOR OGFC AND MEET A MINIMUM TENSILE STRENGTH OF 50 PSI AND A MINIMUM TSR OF 70%.
- (8) INCLUDES 21851 TONS FOR MAINLINE, 684 TONS FOR RAMPS, AND 530 TONS FOR ADDITIONAL COLD PLANING AT BRIDGES WITH VERTICAL CLEARANCE RESTRICTIONS.
- (9) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (10) IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING METHOD APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION).
- (11) CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (12) TO BE USED FOR TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS.
- (13) INCLUDES 1.2 L.M. FOR RAMPS.
- (14) TO BE USED AS PERMANENT PAVEMENT MARKINGS FOR THE EDGELINES AND LANE LINES ON THE FINAL SURFACE.
- (15) USE AN APPROVED TRACKLESS TACK FROM QPL40-F APPLIED WITH A DISTRIBUTOR AT A MINIMUM RATE OF 0.20 GAL/S.Y. (APPROXIMATE RESIDUAL RATE 0.10 GAL/S.Y.). PAVING OF THE OGFC SHALL NOT BEGIN UNTIL THE DEPARTMENT IS SATISFIED THE APPLICATION RATE IS ACHIEVED AND THE EMULSION HAS FULLY BROKEN. MULTIPLE PASSES MAY BE REQUIRED. OR, EMULSION TYPE CQS-1HP MAY BE APPLIED WITH A SPRAY PAVER AT AN APPLICATION RATE BETWEEN 0.18 TO 0.23 GAL/S.Y. THE SPRAY PAVER SHALL BE A SINGLE PIECE OF EQUIPMENT THAT APPLIES THE TACK COAT AND SPREADS THE BITUMINOUS PAVEMENT. AT A MINIMUM THE SPRAY PAVER SHALL MEET THE PAVER REQUIREMENTS OF 407.06 AND THE DISTRIBUTOR REQUIREMENTS IN 402.03.
- (16) TO BE USED FOR TACK COAT UNDERNEATH OGFC. HOT APPLIED TRACKLESS TACK MAY BE EITHER HOT APPLIED ASPHALT BINDER OR AN APPROVED HOT APPLIED TRACKLESS TACK COAT FROM QPL40, SECTION F. IF USING ASPHALT BINDER THE MINIMUM GRADE SHALL BE PG64-22 BUT A HIGHER GRADE MAY BE USED AT THE CONTRACTORS DISCRETION.
- (17) SEE "APPLIED TEXTURE FINISH" TABLE ON SHEET 2E FOR LOCATIONS. THE COLOR IS TO BE "WHITE, FEDERAL SPECIFICATION NO. 37886." COST TO INCLUDE REPLACING BARRIER WALL DELINEATORS.
- (18) ITEM INCLUDES AN EXTRA 240' (20' PER LEAD LINE) THAT IS TO REMAIN IN PULL BOX FOR INSTALLATION BY TRAFFIC CONTROL PERSONNEL.
- (19) TO BE USED FOR GORE AREAS.
- (20) CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.
- (21) SEE SHEETS 2G - 2G7. REFERENCE STANDARD DRAWINGS T-M-9, T-M-9A, AND T-M-9B.
- (22) TO BE USED TO REPLACE AREA DRAIN GRATE AT WB GORE AREA AT LM 17.406 WITH AN ADA COMPLIANT GRATE. ALSO INCLUDES VERTICAL ADJUSTMENT OF EXISTING CATCH BASIN TO MEET ROADWAY GRADE.

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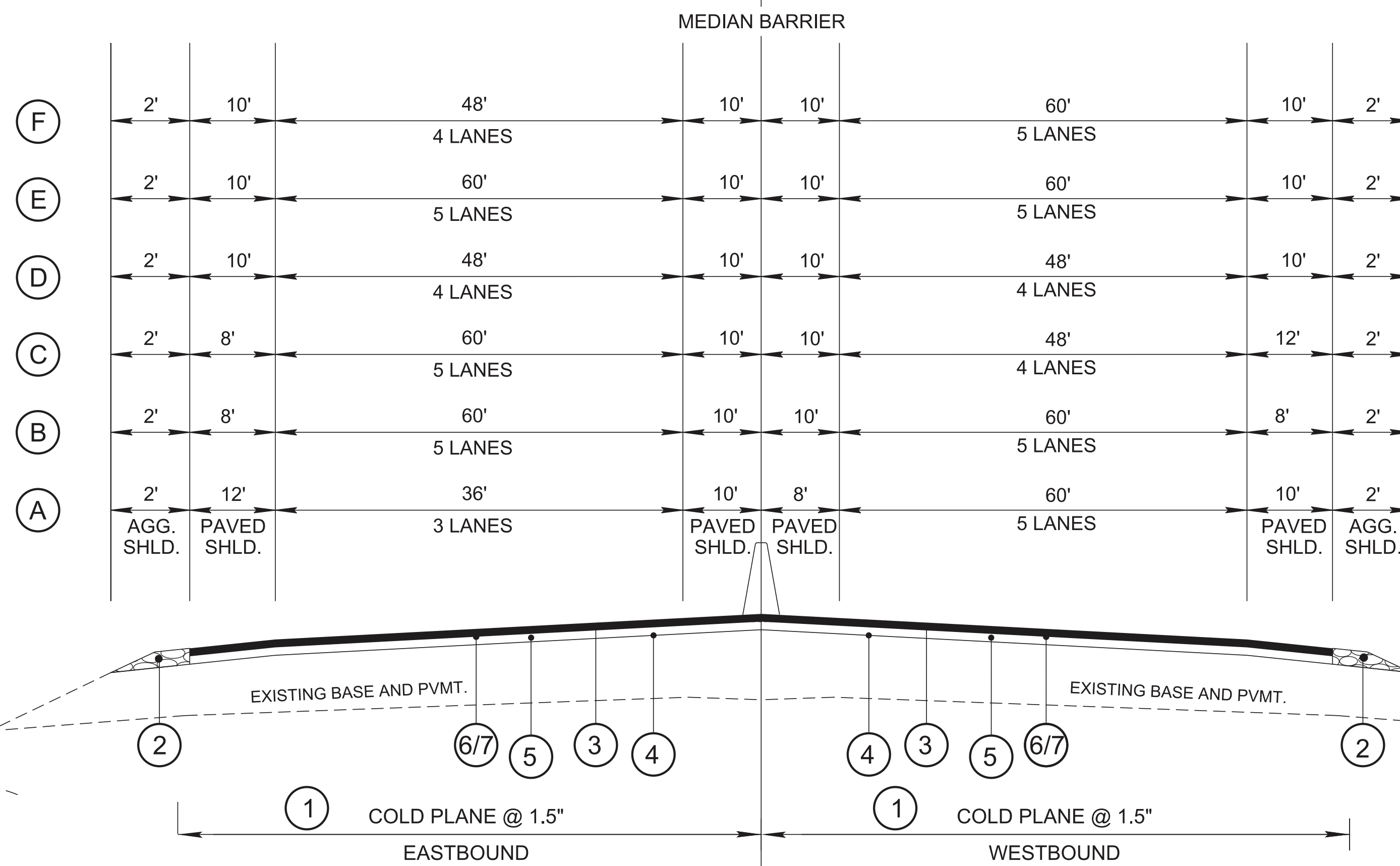


2-22-24

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2A

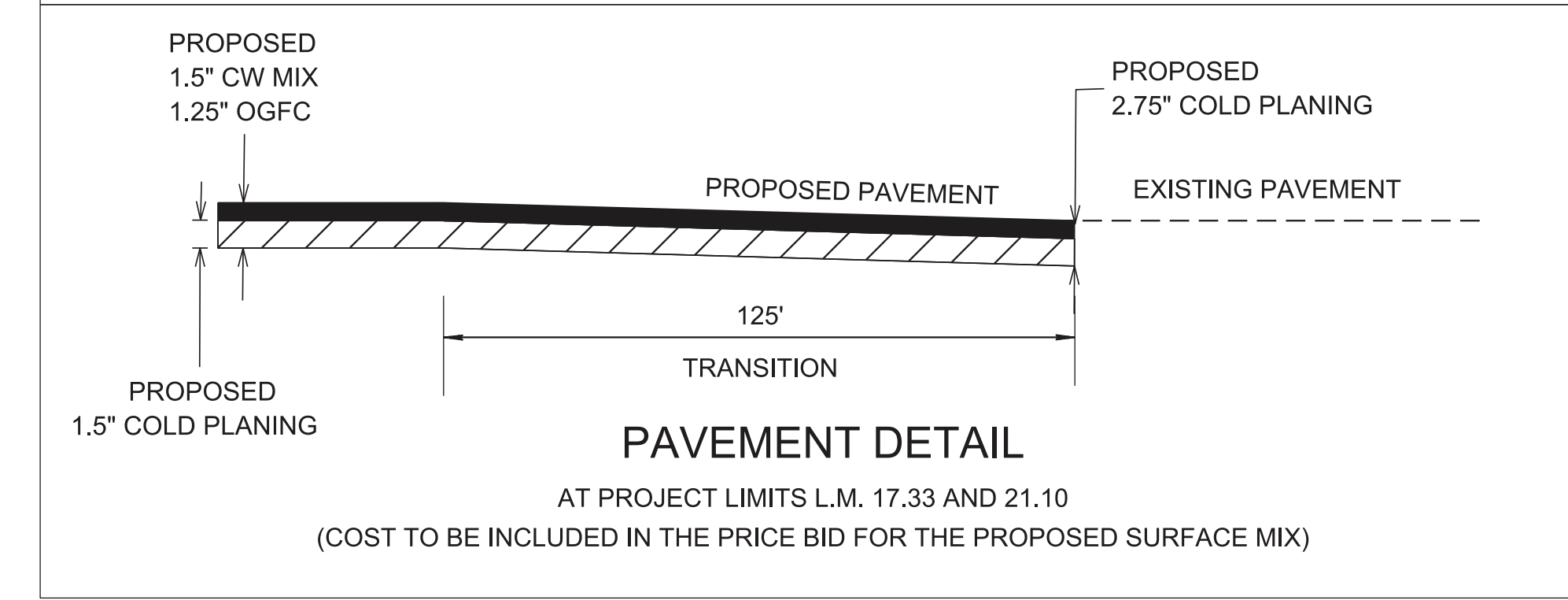
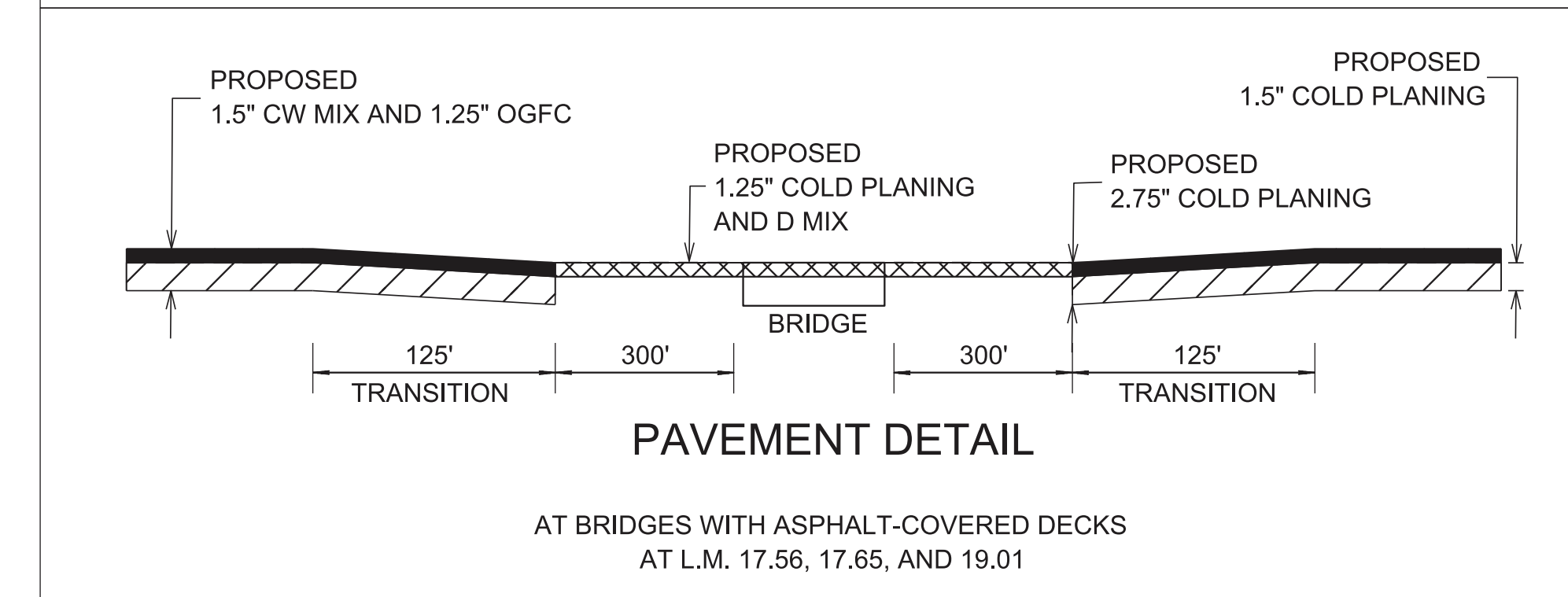
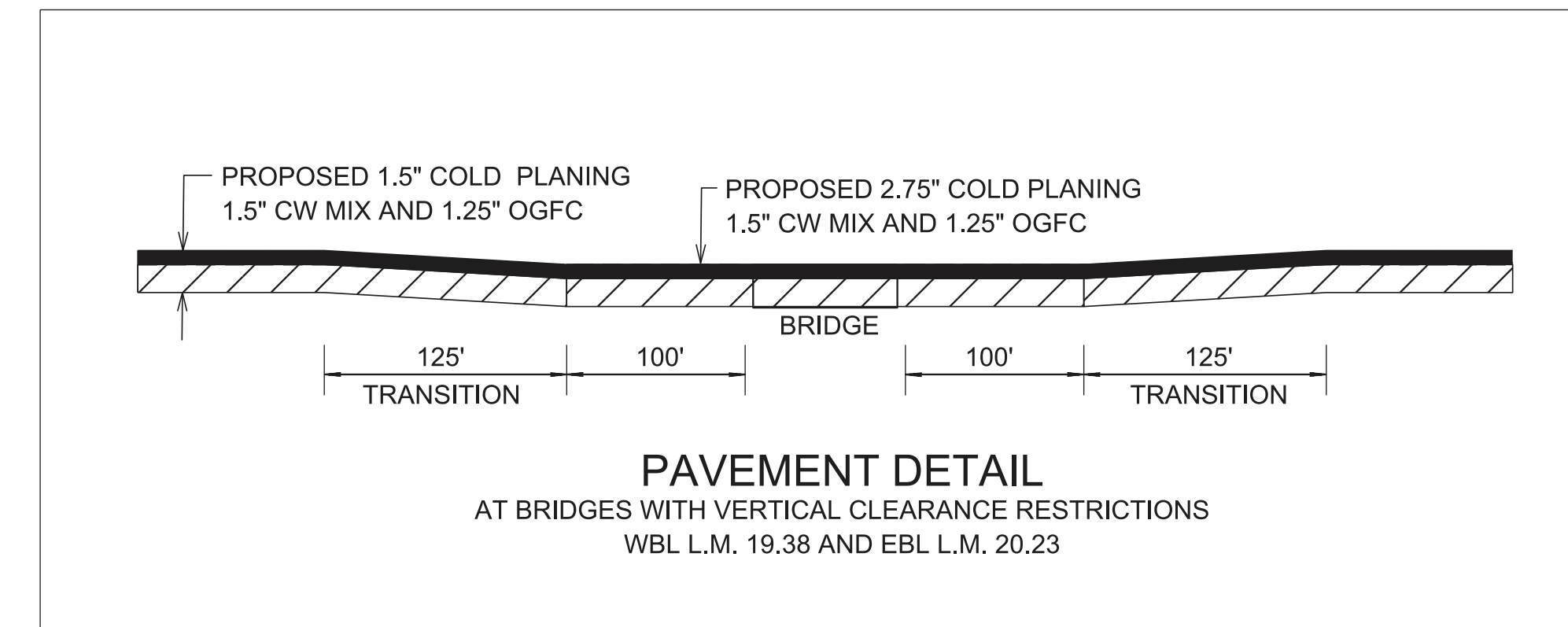


SPECIAL NOTE:
THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

SPECIAL NOTE:
AT THE START OF EACH NIGHTS PAVING PRODUCE APPROXIMATELY 15 TONS OF THE BITUMINOUS PAVEMENT FOR THE PURPOSE OF PLANT START AND HEATING UP THE MATERIAL TRANSFER DEVICE (MTD). THIS MATERIAL SHALL BE UNLOADED INTO THE MTD AT A MINIMUM TEMPERATURE OF 280°F AND THE ENTIRE LOAD DISCHARGED THROUGH THE MTD AND WASTED OFF THE PROJECT SITE IMMEDIATELY PRIOR TO THE COMMENCING PAVING OPERATIONS.

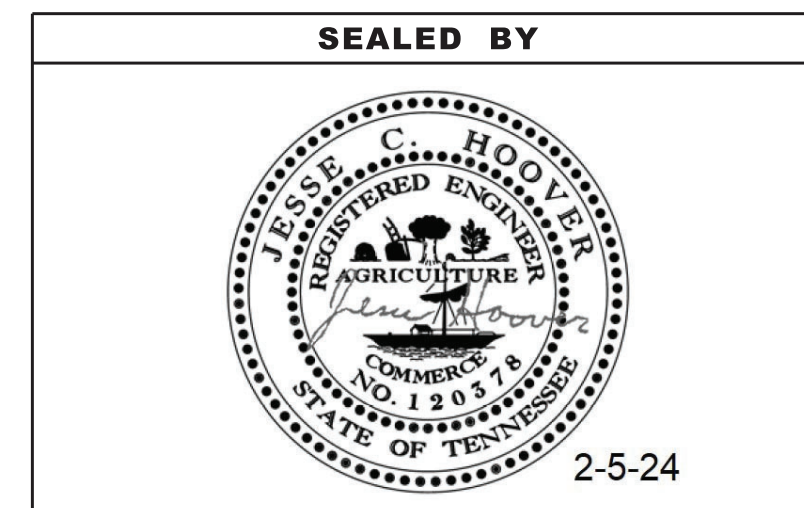
I-24 DAVIDSON COUNTY TYPICAL SECTIONS

- (A) L.M. 17.33 TO 17.55
- (B) L.M. 17.55 TO 17.87
- (C) L.M. 17.87 TO 18.05
- (D) L.M. 18.05 TO 18.17
L.M. 18.27 TO 18.35
L.M. 18.66 TO 19.84
L.M. 20.11 TO 20.43
- (E) L.M. 18.17 TO 18.26
L.M. 18.37 TO 18.56
L.M. 20.43 TO 21.10
- (F) L.M. 18.26 TO 18.27
L.M. 18.35 TO 18.37
L.M. 18.56 TO 18.66
L.M. 19.84 TO 20.11



ALTERNATE AA1 PROPOSED PAVEMENT SCHEDULE	
1	COLD PLANING @ 1.5" THICK (110 LB./S.Y. FOR 1.25", 26 LB./S.Y. FOR 0.25") 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TON)
2	MINERAL AGGREGATE @ 2" THICK 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"
3	SURFACE @ 1.25" THICK (110 LB./S.Y.) 411-03.23 ACS MIX (PG76-22) OGFC
4	TACK COAT (MILLING-COLD PLANE) 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
5	BIT. BINDER @ 1.5" THICK (165 LB./S.Y.) 307-03.11 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING CW
6	TRACKLESS TACK COAT (0.20 GAL/S.Y.) 403-02.01 TRACKLESS TACK COAT

ALTERNATE AA2 PROPOSED PAVEMENT SCHEDULE	
1	COLD PLANING @ 1.5" THICK (110 LB./S.Y. FOR 1.25", 26 LB./S.Y. FOR 0.25") 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TON)
2	MINERAL AGGREGATE @ 2" THICK 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"
3	SURFACE @ 1.25" THICK (110 LB./S.Y.) 411-03.23 ACS MIX (PG76-22) OGFC
4	TACK COAT (MILLING-COLD PLANE) 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
5	BIT. BINDER @ 1.5" THICK (165 LB./S.Y.) 307-03.11 ASPHALT CONCRETE MIX (PG76-22) (BPMB-HM) GRADING CW
7	HOT APPLIED TACK COAT (0.13 - 0.18 GAL/S.Y.) 403-02.02 HOT APPLIED TACK COAT (TC)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

23-JAN-2024 11:24 \\dot03nas002.tdot.state.tn.us\03Shared\SURVEY\DESIGN\Resurfacing Plans\2024\Interstate\Davidson Co. I-24 L.M. 17.33 - 21.10\2A-131712-00-TypicalSections.sht

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-1-24-1(140)	2B

GENERAL NOTES

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) IN AREAS WITH OPEN-GRADED FRICTION COURSE PAVEMENT, THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED IN A MANNER WHICH PERMITS PROPER DRAINAGE OF PAVEMENT STRUCTURE. MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

SIGNALIZATION

- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (7) IF RESURFACING IS INCLUDED IN THE PROJECT, SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.

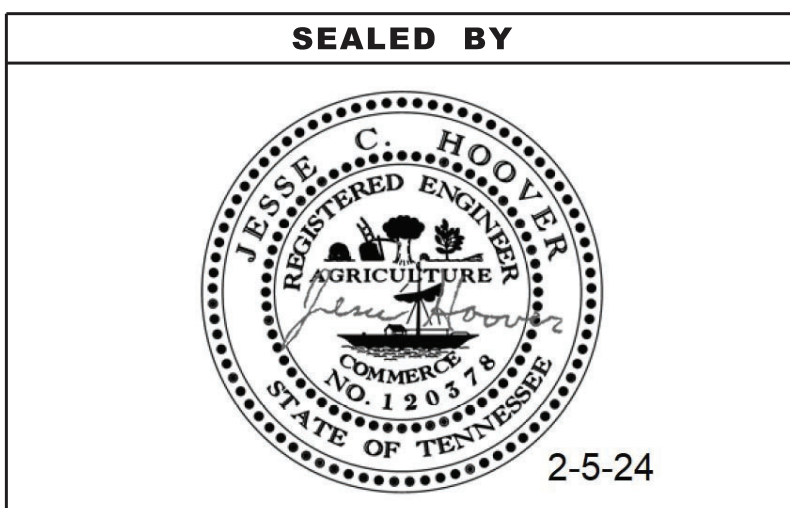
CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.

- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

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SPECIAL NOTES

MISCELLANEOUS

- (1) THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS TO NOT DAMAGE THE EXISTING DECK, SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

PAVEMENT MARKINGS

- (2) THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT. EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

PAVING

- (3) SURFACE TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (4) AT THE START OF EACH NIGHTS PAVING PRODUCE APPROXIMATELY 15 TONS OF THE BITUMINOUS PAVEMENT FOR THE PURPOSE OF PLANT START UP AND HEATING UP THE MATERIAL TRANSFER DEVICE (MTD). THIS MATERIAL SHALL BE UNLOADED INTO THE MTD AT A MINIMUM TEMPERATURE OF 280°F AND THE ENTIRE LOAD DISCHARGED THROUGH THE MTD AND WASTED OFF THE PROJECT SITE IMMEDIATELY PRIOR TO THE COMMENCING PAVING OPERATIONS.

RESURFACING

- (5) TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:
 1. THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.
 2. THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.
 3. THE DIFFERENCE IN ELEVATION BETWEEN THE MILLED SURFACE AND THE ADJACENT LANE SHALL NOT EXCEED 1 1/2 INCHES.
 4. THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.
 5. RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.
 6. ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (TN-64) PLACED IN ADVANCE OF ANY MILLED AREAS.
 7. IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED
 8. ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (6) DURING COLD PLANING OPERATION, A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION WILL REMAIN OPEN.
- (7) AFTER ANY COLD PLANING OPERATION, TEMPORARY STRIPING WILL BE REQUIRED BEFORE RETURNING TRAFFIC ONTO EXCAVATED ROADWAY.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (8) THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) ONE ADVANCE ROAD CONSTRUCTION AHEAD SIGN IS TO BE PLACED ON ALL RAMPS PRIOR TO MERGING WITH THE MAINLINE. LOCATION TO BE DETERMINED IN THE FIELD BY THE ENGINEER OR CONTRACTORS SUPERINTENDENT.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (10) DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:
 1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
 - a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
 - d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
 2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
 - a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
 - (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
 - (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
 - b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
 - c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

TRAFFIC COUNTERS

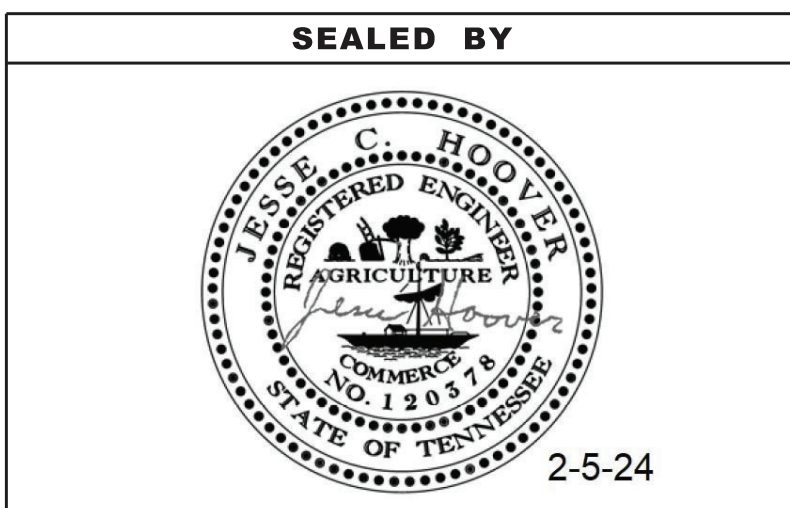
- (11) THE CONTRACTOR SHALL SUPPLY AND INSTALL ALL RELATED EQUIPMENT. THE CONTRACTOR SHALL MAKE SURE EACH DETECTION LOOP IS OPERATIONAL AT THE COMPLETION OF THE PROJECT. THE STATE'S LONG RANGE PLANNING DIVISION'S TRAFFIC COUNTER PERSONNEL WILL MAKE THE TRAFFIC COUNTING SITE FULLY USEABLE.
- (12) ALL LOOPS SHALL BE 5' X 7' FOOT TYPICAL IN SIZE WITH 3 TURNS OF LOOP WIRE. TWO LOOPS LOCATED I LEAD WIRES TO ALL LOOPS SHALL BE RUN THROUGH 2" PVC CONDUIT FROM THE ROAD TO AN IN-GROUND PULL BOX, AN EXTRA 20 FOOT OF WIRE SHOULD BE STORED INSIDE THE PULL BOX AND THE LOOP LEADS SHALL BE RUN THROUGH CONDUIT AND PLACED INSIDE THE CABINET. EACH LOOP LEAD-INS SHALL BE MARKED WITH LANE IDENTIFICATION, DENOTING THE LANE AND POSITION IN THE LANE FOR EACH LOOP (SUCH AS NB-1 FOR NORTH BOUND ETC.).
- (13) COUNTER CABINET SHALL NOT BE PRE-WIRED FOR A TRAFFIC SIGNAL AND SHALL INCLUDE VENTILATION THROUGH THE TOP AND DOOR BUT DOES NOT TO INCLUDE A VENTILATION FAN.
- (14) MINIMUM SIZE OF CABINET SHALL BE 41" TALL, 25" WIDE AND 18" DEEP AND INCLUDE ONE ADJUSTABLE SIDE SHELF. COMPLETE INSTALLATION PER SPECIAL TRAFFIC COUNTER SPECIFICATION NOTES CONCERNING THE COUNTER CABINET. ITEM INCLUDES THE 3" GALVANIZED, RIGID CONDUIT POLE THAT THE CABINET IS TO BE MOUNTED ONTO. THE CABINET DOOR SHALL BE SEALED WITH A CLOSED CELL NEOPREME GASKET BONDED TO THE INSIDE OF THE DOOR WITH AN OIL RESISTENT ADHESIVE THAT WILL BE POSITIONED TO PREVENT ANYTHING FROM PENETRATING THROUGH THE CRACK BETWEEN THE DOOR AND THE CABINET WHEN THE DOOR IS CLOSED. CABINET SHALL INCLUDE A THREE POINT LOCKING SYSTEM AND BE MOUNTED SO THAT THE CABINET DOOR WILL OPEN TOWARD THE ROADWAY.
- (15) THE CABINET SHALL BE POLE-MOUNTED TO A 3" GALVANIZED, RIGID CONDUIT POLE WITH THE BOTTOM OF THE CABINET AT LEAST 36" ABOVE GROUND LEVEL. TOP OF THIS POLE SHALL BE AT LEAST EIGHT (8') FEET ABOVE THE GROUND. A 65 WATT SOLAR PANEL SHALL BE MOUNTED ON THE 3" RIGID CONDUIT, ALL SOLAR PANEL WIRING SHALL BE RUN THROUGH A WEATHERHEAD AND CONDUIT DOWN THE POLE TO INSIDE THE CABINET AND TERMINATED TO A VOLTAGE REGULATOR. A 12 VOLT 70-90 AMP HOUR DEEP DISCHARGE MARINE BATTERY SHALL BE INCLUDED.
- (16) PLEASE CONTACT STANLEY DUNN AT 615-350-4571 OR 615-210-0431 WHEN ALL TRAFFIC LOOPS ARE INSTALLED AND READY FOR FINAL WIRING INSTALLATION BY TDOT PERSONNEL.

CSXT SAFETY NOTES

- (17) COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (18) ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE DONE ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (19) ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (20) CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY.
- (21) ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (22) ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (23) AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (24) THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2C

REV. 01-23-24: ADDED CSXT SAFETY NOTES.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

SPECIAL NOTES (CONT.)

CSXT SAFETY NOTES (CONT.)

- (2) THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT'S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS.
- (3) PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD'S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (4) ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (5) NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (6) ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORK DAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (7) ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (8) "ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
- (9) ALL SOILS EXCAVATED WITHIN CSXT'S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT'S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.
- (10) THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE
CROUCH ENGINEERING, INC.

5115 MARYLAND WAY, SUITE 225
BRENTWOOD, TN 37027
ATTN: MR. SCOTT VICK, P.E. PROJECT DIRECTOR
PHONE: (615) 791-0630
EMAIL: SVICK@CROUCHENGINEERING.COM

UTILITY

- (11) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (12) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (13) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS.
- (14) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS (UPDATED 08-11-2023)

TELEPHONE & FIBER:

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CABLE:

COMCAST (XFINITY)
660 MAINSTREAM DRIVE
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DERRICK CUNNINGHAM
DERRICK_CUNNINGHAM@COMCAST.COM
NAS-NASHVILLE.CONSTRUCTIONBETTERMENTS@COMCAST.COM
C: 615-878-4237

WATER & SEWER:

METRO WATER & SEWER SERVICE
1600 SECOND AVENUE NORTH
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O:615-862-4534

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ELECTRIC:

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TENNESSEE VALLEY AUTHORITY
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2C1

REV. 01-23-24: ADDED CSXT SAFETY NOTES.

SEALED BY



2-5-24

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING, PAVING, STRIPING, TRAFFIC CONTROL, AND OTHER TASKS AS INDICATED IN THE PLANS OR AS DIRECTED BY THE TDOT ENGINEER.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

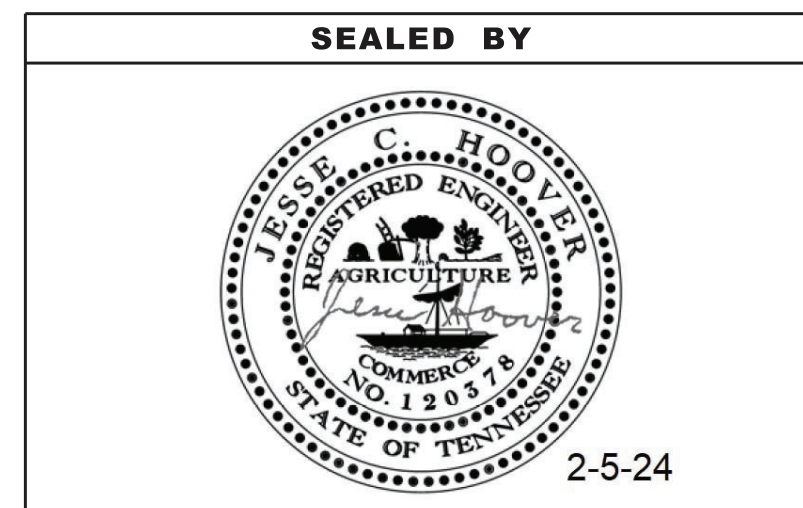
- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2D



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**ENVIRONMENTAL
NOTES**

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ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

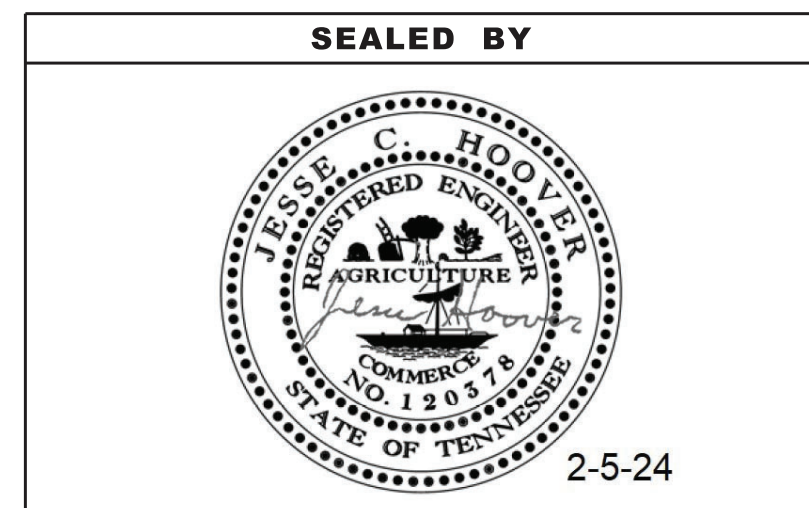
EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

RAILROAD ENVIRONMENTAL

- (15) THE CONTRACTOR SHALL MAINTAIN A COMPLETE AND COMPREHENSIVE PLAN TO PREVENT ROADWAY AND/OR CONSTRUCTION SEDIMENT OR DEBRIS AND ANY PETROLEUM BASED PRODUCTS OR CHLORINATED SOLVENTS, PAINTS OR COATINGS ETC. FROM FALLING ONTO THE RAILROAD'S RIGHT-OF-WAY AND/OR FROM ENTERING THE DRAINAGE DITCHES OR DRAINAGE STRUCTURES OF THE RAILROAD, AND ANY SEDIMENT OR DEBRIS OR PETROLEUM BASED PRODUCTS OR CHLORINATED SOLVENTS, ETC. THAT DO ENTER SUCH DRAINAGE AREAS OF THE RAILROAD'S RIGHT-OF-WAY ARE TO BE REMOVED IN ACCORDANCE WITH RULES SET FORTH BY CSXT AND AT THE CONTRACTOR'S EXPENSE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2D1

REV. 01-23-24: ADDED RAILROAD ENVIRONMENTAL NOTE.



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**ENVIRONMENTAL
NOTES**

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2E

REV. 02-21-24: ADDED "APPLIED TEXTURE FINISH" TABLE.

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES		S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x W			
E5-1	EXIT GORE	72"	X 60"	30	5	150
E5-2	EXIT OPEN	48"	X 36"	12	5	60
G20-1	ROAD WORK NEXT 6 MILES	48"	X 24"	8	2	16
G20-2	END ROAD WORK	48"	X 24"	8	8	64
W3-2	YIELD AHEAD SYMBOL	48"	X 48"	16	6	96
W4-2R	RIGHT LANE ENDS SYMBOL	48"	X 48"	16	2	32
W4-2L	LEFT LANE ENDS SYMBOL	48"	X 48"	16	2	32
W8-11	UNEVEN LANES	48"	X 48"	16	14	224
W8-15	GROOVED PAVEMENT	48"	X 48"	16	14	224
W8-15P	MOTORCYCLE WARNING PLAQUE	30"	X 24"	5	14	70
W20-1	ROAD WORK AHEAD	48"	X 48"	16	6	96
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	X 48"	16	2	32
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	X 48"	16	2	32
W20-5R	RIGHT 2 LANES CLOSED 1/2 MILE	48"	X 48"	16	2	32
W20-5R	RIGHT 2 LANES CLOSED 1500 FT	48"	X 48"	16	2	32
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	X 48"	16	2	32
W20-5L	LEFT LANE CLOSED 1500 FT	48"	X 48"	16	2	32
W20-5L	LEFT 2 LANES CLOSED 1/2 MILE	48"	X 48"	16	2	32
W20-5L	LEFT 2 LANES CLOSED 1500 FT	48"	X 48"	16	2	32
W21-2	FRESH OIL - PORTABLE	48"	X 48"	16	2	32
W4-1R	MERGE NOW (SPECIAL)	48"	X 48"	16	6	96
R1-2	YIELD	48"	X 48"	16	6	96
TOTAL					1544	


THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

NO GUARDRAIL INCLUDED ON THIS PROJECT

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
19100240011	17.560	OVER OLD GLENROSE AVE. / CSXT [349217F]	254'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT.
19100240013	17.650	OVER GLENROSE AVE.	194'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT.
19100240015	18.030	UNDER THOMPSON LN.		VERTICAL CLEARANCE: EBL = 20'-10", WBL = 20'
19100240017	18.190	UNDER SR-155 BRILEY PKWY.		VERTICAL CLEARANCE: EBL = 21'-1", WBL = 17'-3"
19100240021	19.010	OVER SEVENMILE CREEK	135'	COLD PLANE 1.25" OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW ASPHALT.
19100240023	19.260	UNDER ANTIOCH PK.		VERTICAL CLEARANCE: EBL = 16'-11", WBL = 17'-6"
19100240025	19.380	UNDER CSXT [340974V]		VERTICAL CLEARANCE: EBL = 19'-6", WBL = 16'-2"
19100240027	20.230	UNDER SR-255 HARDING PL.		VERTICAL CLEARANCE: EBL = 16'-3", WBL = 16'-7"

APPLIED TEXTURE FINISH			
LOG MILE	EASTBOUND / WESTBOUND	LOCATION DESCRIPTION	AREA (S.Y.)
17.59 - 17.73	WESTBOUND	36" OUTSIDE WALL	104
19.00 - 19.05	WESTBOUND	36" OUTSIDE WALL	90
17.56 - 17.69	EASTBOUND	36" OUTSIDE WALL	102
17.86 - 17.91	EASTBOUND	51" OUTSIDE WALL	63
18.00 - 18.09	EASTBOUND	51" BARRIER/RETAINING WALL ALONG RAMP/BRILEY PARKWAY	1091
19.01 - 19.04	EASTBOUND	36" OUTSIDE WALL	57
17.33 - 21.10	CENTER	51" CENTER MEDIAN BARRIER WAL	5456
TOTAL			6963

SEALED BY



2-22-24

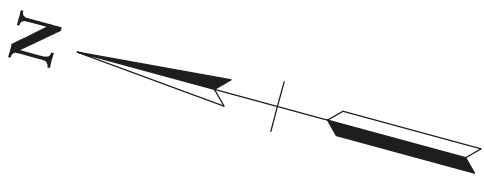
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

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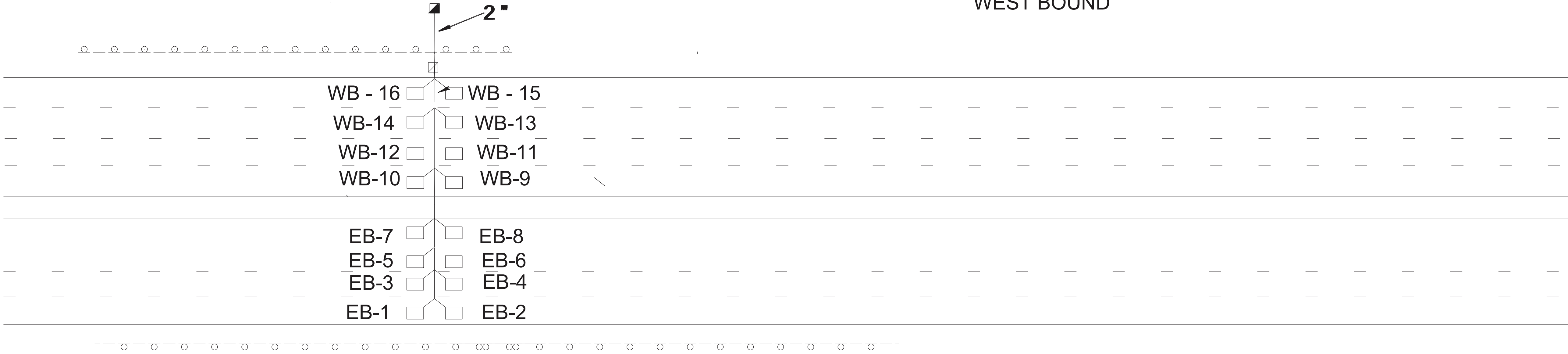
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-1-24-1(140)	2F

NOTE: NOTIFICATION OF COMPLETION AND THE X/Y COORDINATES OF ALL PULL BOXES IS REQUIRED SEE SPECIAL TRAFFIC COUNTER SPECIFICATIONS



COUNTER CABINET

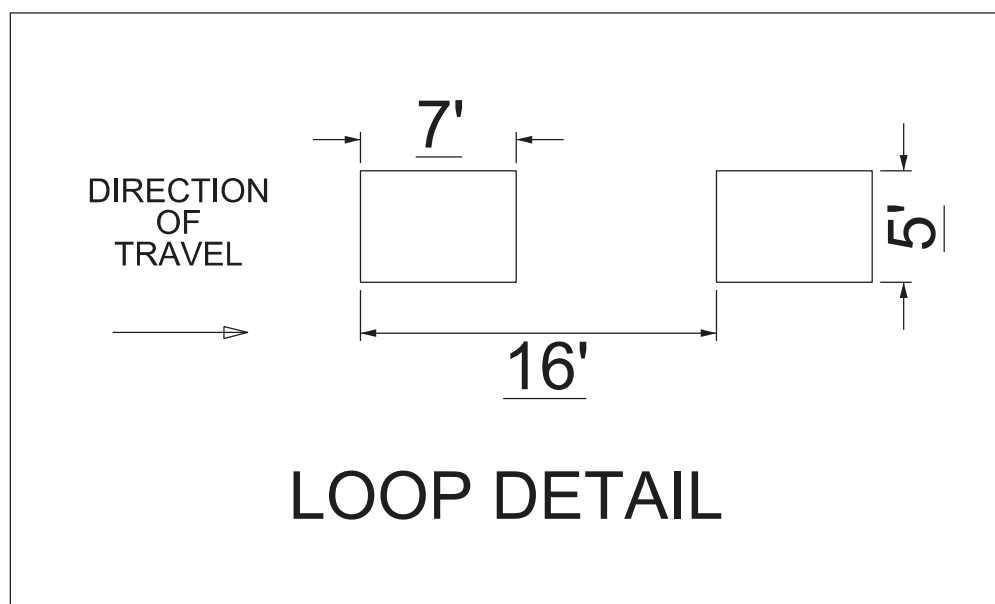
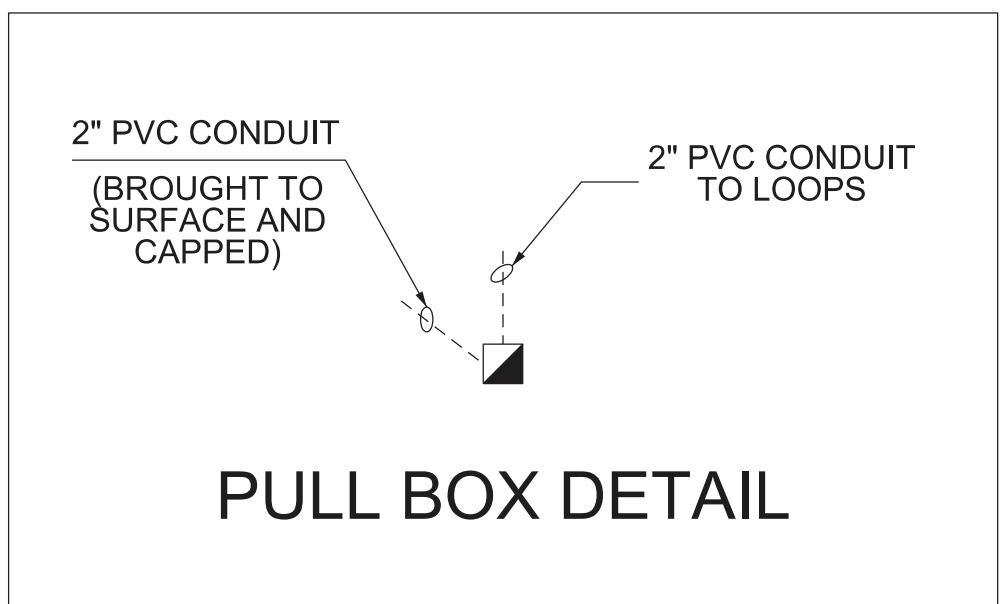
WEST BOUND



EAST BOUND

DAVIDSON COUNTY
I-24
L.M. 19.325

NOT TO SCALE



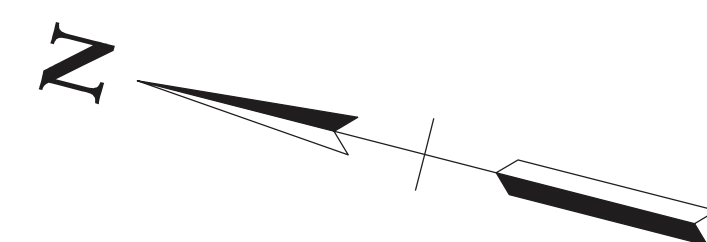
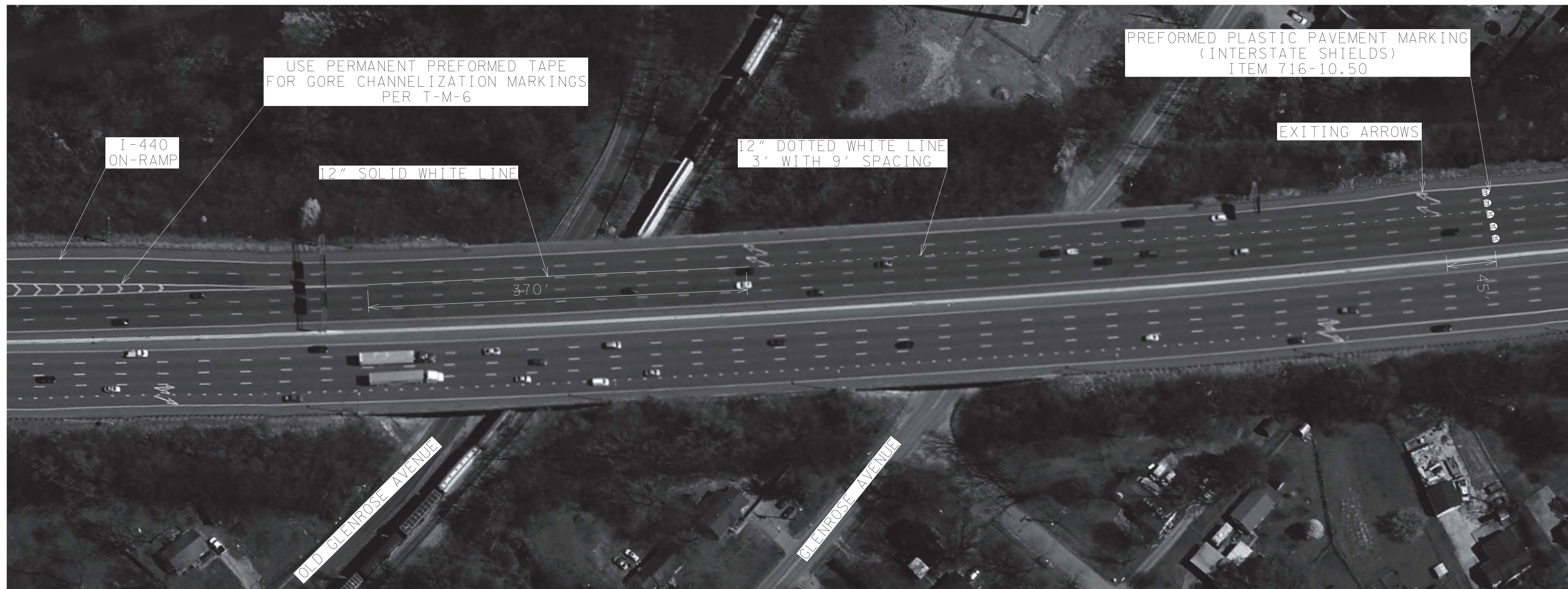
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DETAIL SHEET

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G



NOT TO SCALE

INTERSTATE 24
 17.334 LOG MILE TO 17.796 LOG MILE
 DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
 TDOT REGION 3 TRAFFIC

REFERENCE STANDARD DRAWINGS
 T-M-6

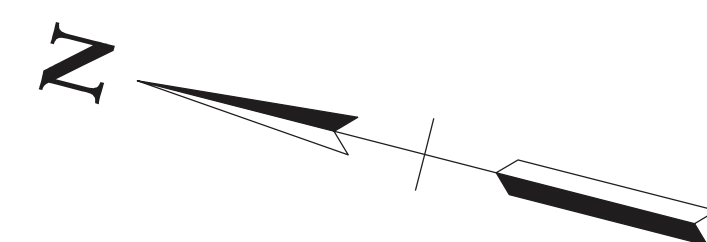
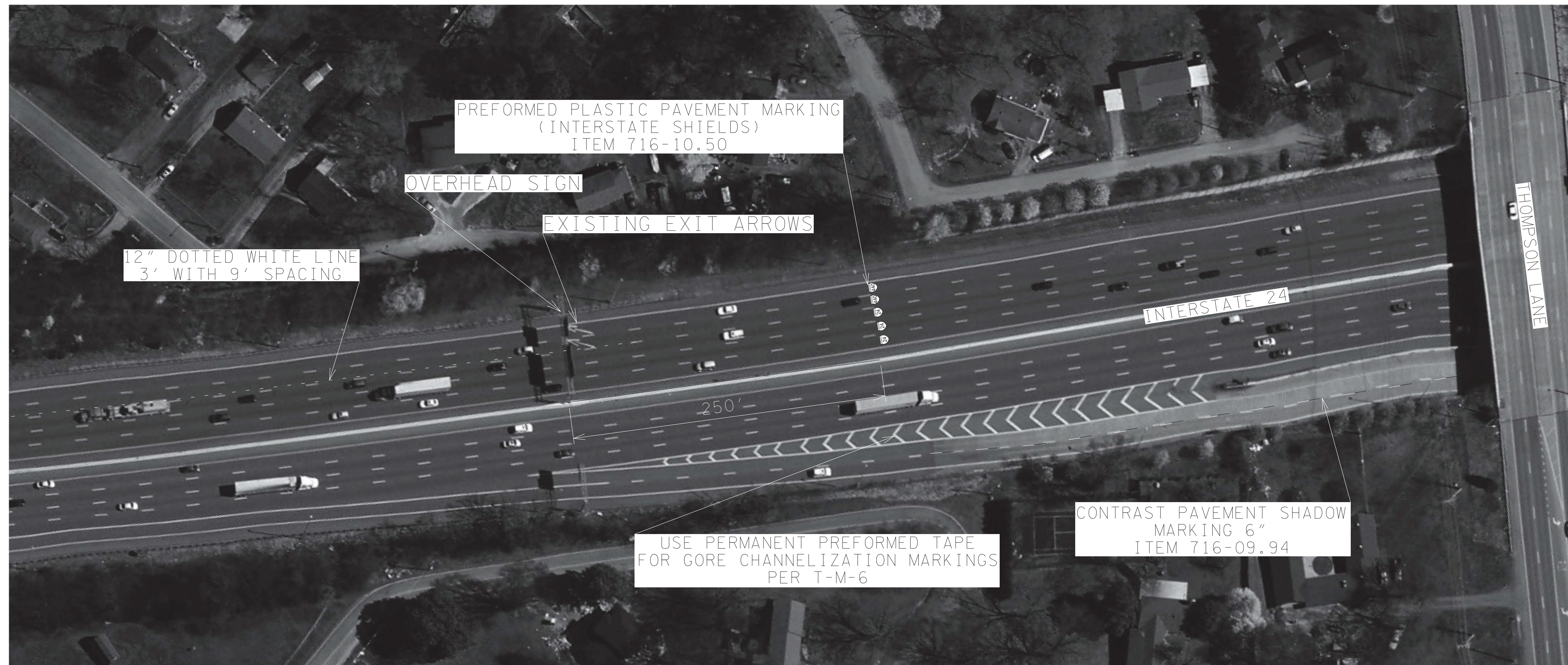
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PAVEMENT
 MARKING
 IMPROVEMENTS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G1



NOT TO SCALE

INTERSTATE 24
17.796 LOG MILE TO 18.047 LOG MILE
DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
TDOT REGION 3 TRAFFIC

REFERENCE STANDARD DRAWINGS
T-M-6

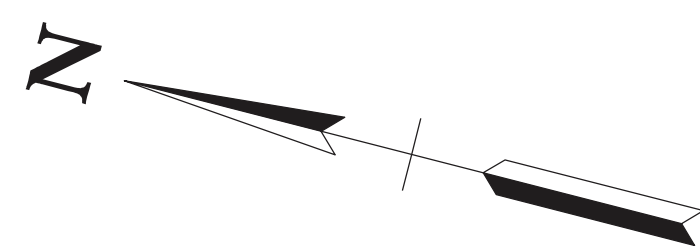
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G2



INTERSTATE 24
 18.047 LOG MILE TO 18.269 LOG MILE
 DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
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REFERENCE STANDARD DRAWINGS
 T-M-6, T-M-9, T-M-9A, AND T-M-9B

NOT TO SCALE

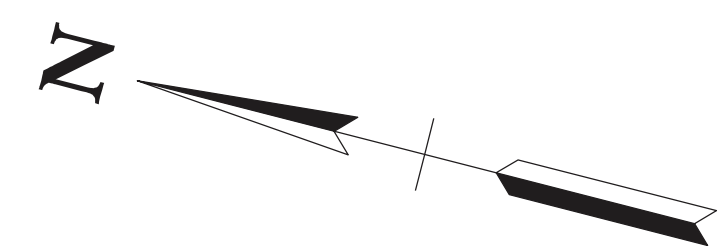
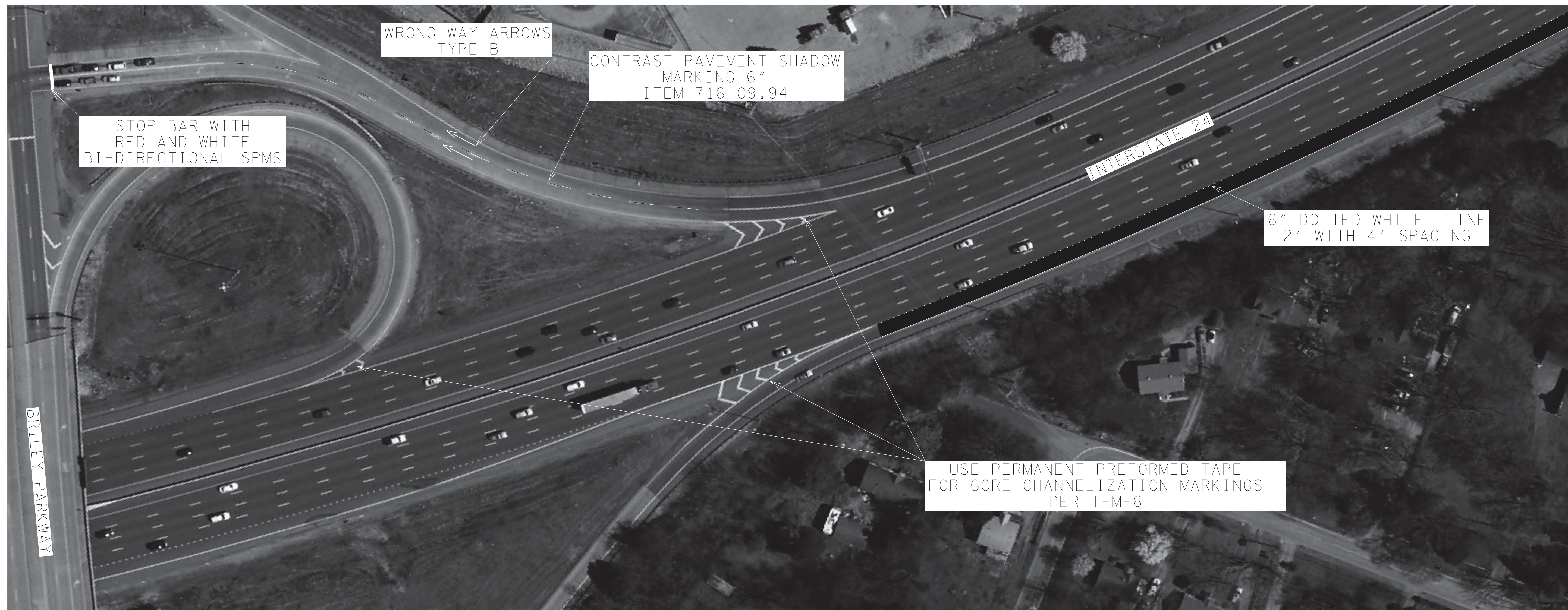
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PAVEMENT
 MARKING
 IMPROVEMENTS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G3



NOT TO SCALE

INTERSTATE 24
18.269 LOG MILE TO 18.480 LOG MILE
DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
TDOT REGION 3 TRAFFIC

REFERENCE STANDARD DRAWINGS
T-M-6, T-M-9, T-M-9A, AND T-M-9B

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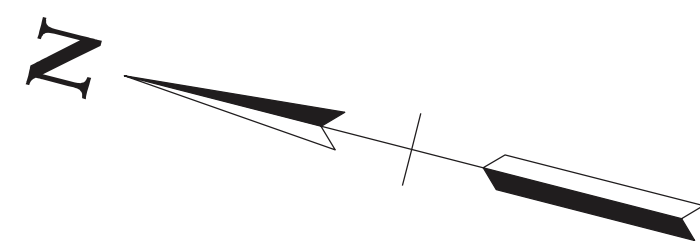
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G4



6" DOTTED WHITE LINE
2' WITH 4' SPACING



NOT TO SCALE

INTERSTATE 24
18.480 LOG MILE TO 18.803 LOG MILE
DAVIDSON COUNTY

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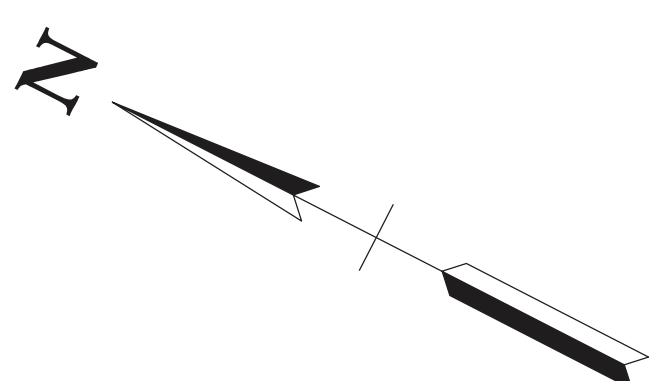
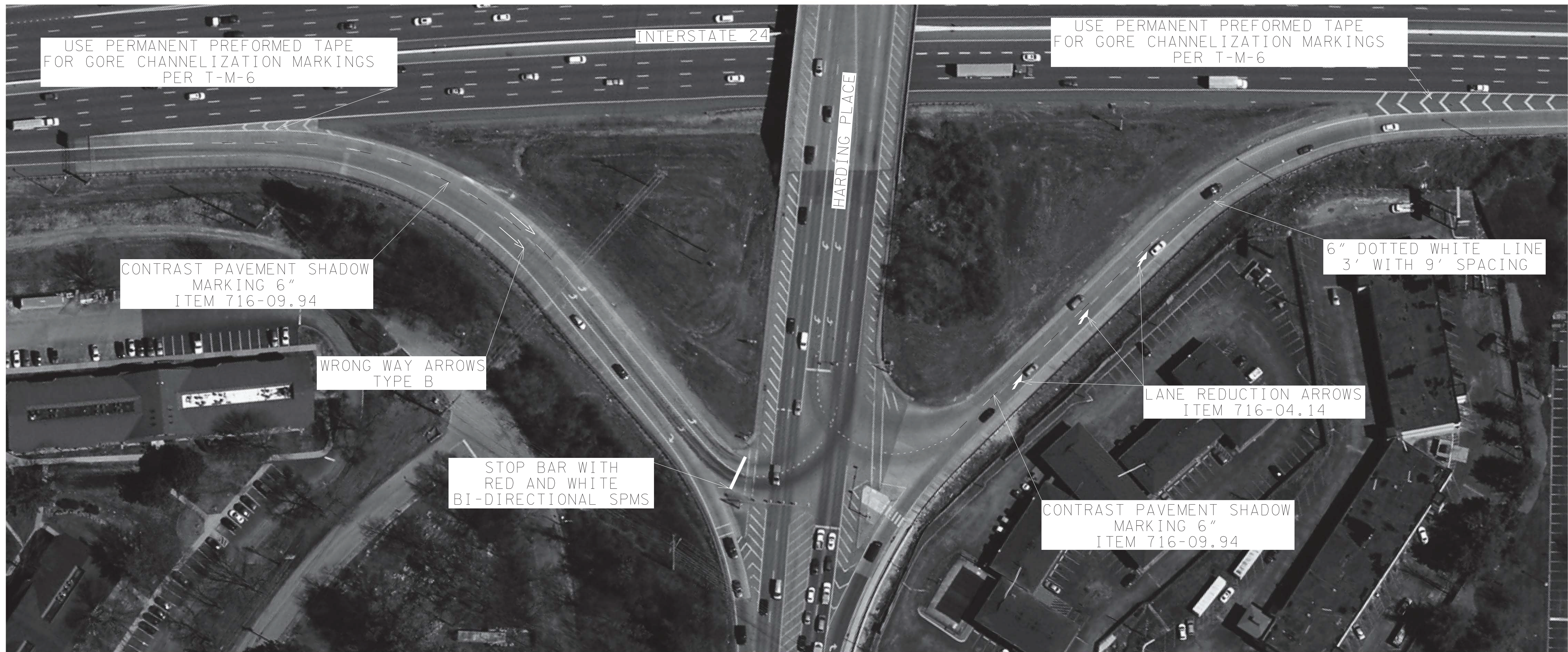
REFERENCE STANDARD DRAWINGS
T-M-6

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PAVEMENT
MARKING
IMPROVEMENTS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G5



NOT TO SCALE

INTERSTATE 24
 20.075 LOG MILE TO 20.417 LOG MILE
 WEST SIDE OF THE INTERCHANGE
 DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
 TDOT REGION 3 TRAFFIC

REFERENCE STANDARD DRAWINGS
 T-M-5, T-M-6, T-M-9, T-M-9A,
 AND T-M-9B

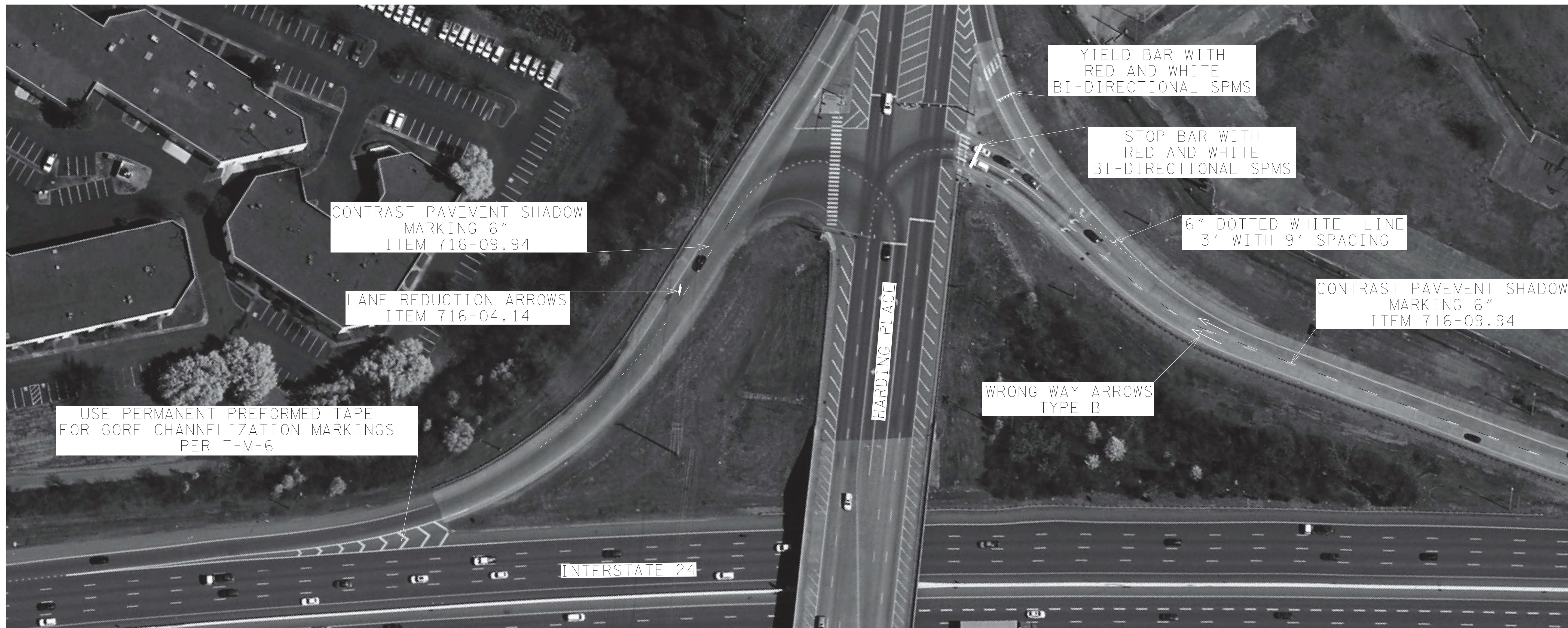
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PAVEMENT
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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G6



USE PERMANENT PREFORMED TAPE FOR GORE CHANNELIZATION MARKINGS PER T-M-6

CONTRAST PAVEMENT SHADOW MARKING 6" ITEM 716-09.94

LANE REDUCTION ARROWS ITEM 716-04.14

YIELD BAR WITH RED AND WHITE BI-DIRECTIONAL SPMS

STOP BAR WITH RED AND WHITE BI-DIRECTIONAL SPMS

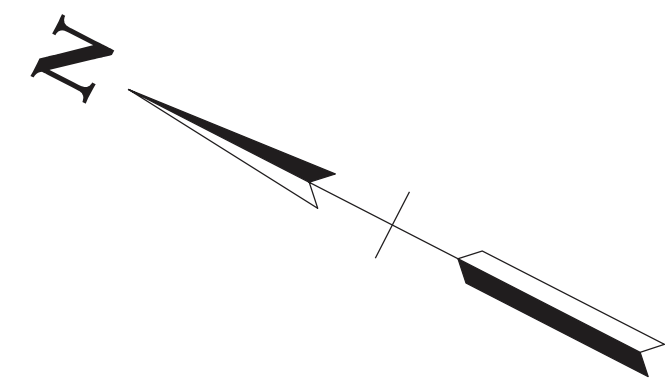
6" DOTTED WHITE LINE 3' WITH 9' SPACING

CONTRAST PAVEMENT SHADOW MARKING 6" ITEM 716-09.94

WRONG WAY ARROWS TYPE B

INTERSTATE 24

HARDING PLACE



INTERSTATE 24
20.075 LOG MILE TO 20.417 LOG MILE
EAST SIDE OF THE INTERCHANGE
DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY TDOT REGION 3 TRAFFIC

REFERENCE STANDARD DRAWINGS
T-M-5, T-M-6, T-M-9, T-M-9A,
AND T-M-9B

NOT TO SCALE

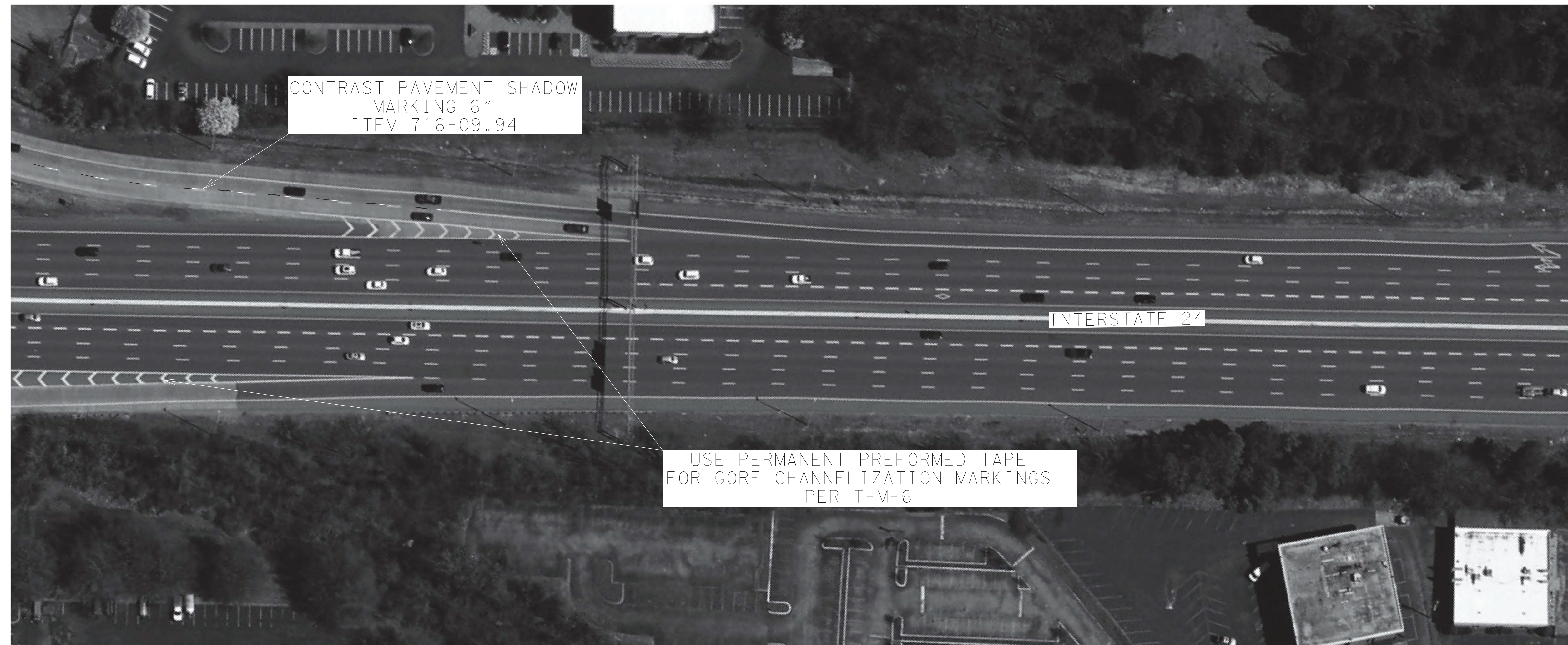
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PAVEMENT
MARKING
IMPROVEMENTS

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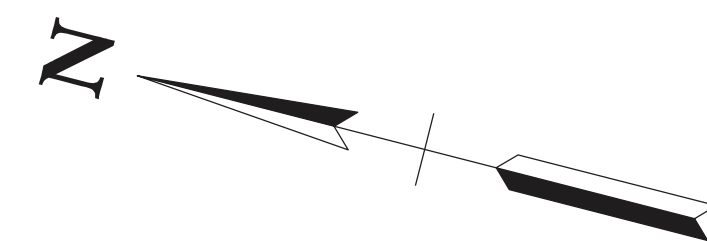
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2024	NH-I-24-1(140)	2G7



CONTRAST PAVEMENT SHADOW
MARKING 6"
ITEM 716-09.94

INTERSTATE 24

USE PERMANENT PREFORMED TAPE
FOR GORE CHANNELIZATION MARKINGS
PER T-M-6



NOT TO SCALE

INTERSTATE 24
20.417 LOG MILE AND 20.615 LOG MILE
DAVIDSON COUNTY

FOR REFERENCE ONLY PROVIDED BY
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REFERENCE STANDARD DRAWINGS
T-M-6

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PAVEMENT
MARKING
IMPROVEMENTS